

Multiple Dwelling Unit Type and Related and Accessory Uses	Automobile Spaces Required Per Dwelling Unit (Unless Otherwise Indicated)			Motorcycle Spaces Required Per Dwelling Unit	Bicycle <sup>(5)</sup> Spaces Required Per Dwelling Unit
	Basic <sup>(1)</sup>	Transit Area <sup>(2)</sup> or Very Low Income <sup>(3)</sup>	Parking Impact <sup>(4)</sup>		
Studio up to 400 square feet	1.25	1.0	1.5	0.05	0.3
1 bedroom or studio over 400 square feet	1.5	1.25	1.75	0.1	0.4
2 bedrooms	2.0	1.75	2.25	0.1	0.5
3-4 bedrooms	2.25	2.0	2.5	0.1	0.6
5+ bedrooms	2.25	2.0	(See footnote 6)	0.2	1.0
Condominium conversion <sup>(8)</sup> 1 bedroom or studio over 400 Square feet 2 bedrooms 3 + bedrooms	1.0	0.75	1.25	N/A	N/A
	1.25	1.0	1.5	N/A	N/A
	1.5	1.25	1.75	N/A	N/A
Rooming and boarding house	1.0 per boarder	0.75 per boarder	1.0 per boarder	0.05 per boarder	0.30 per boarder
Residential care facility (6 or fewer persons)	1 per 3 beds or per permit	1 per 4 beds or per permit	1 per 3 beds or per permit	N/A	N/A
Transitional Housing (6 or fewer persons)	1 per 3 beds or per permit	1 per 4 beds or per permit	1 per 3 beds or per permit	N/A	N/A
Accessory uses (Spaces per square feet <sup>(7)</sup> )	Retail Sales: 2.5 per 1,000	Retail Sales: 2.5 per 1,000	Retail Sales: 2.5 per 1,000	N/A	N/A
	Eating and Drinking Estb.: 5 per 1,000	Eating and Drinking Estb.: 5 per 1,000	Eating and Drinking Estb.: 5 per 1,000		

Footnotes for Table 142-05C

- <sup>1</sup> Basic. The basic parking ratio applies to *development* that does not qualify for the *transit area* parking ratio or the *very low income* parking ratio and that is at least partially within a Parking Impact Area as described in Chapter 13, Article 2, Division 8 (Parking Impact Overlay Zone). *Development* qualifying for either the *transit area* or *very low income* parking ratio that are also within a Parking Impact Area shall also use the basic parking ratio.
- <sup>2</sup> Transit Area. The *transit area* parking ratio applies to *development* that is at least partially within a *transit area* as described in Chapter 13, Article 2, Division 10 (Transit Area Overlay Zone) or that is subject to Chapter 13, Article 2, Division 11 (Urban Village Overlay Zone).

- 3 *Very Low Income.* The *very low income* parking ratio applies to dwelling units limited to occupancy by *very low income* households and *development* covered by an agreement with the San Diego Housing Commission pursuant to Chapter 14, Article 3, Division 7 (Affordable Housing Density Bonus Regulations).
- 4 *Parking Impact.* The parking impact ratio applies to *development* that is at least partially within a designated beach impact area or a campus impact area as described in Chapter 13, Article 2, Division 8 (Parking Impact Overlay Zone), unless otherwise noted.
- 5 *Bicycle.* Bicycle racks are not required for a dwelling unit with a garage accessible only by residents of the dwelling unit.
- 6 *5+ Bedrooms in Parking Impact Areas.* Beach impact area: 2.5 spaces per dwelling unit. Campus impact area: 1 space per bedroom.
- 7 *Accessory Uses.* Square footage includes *gross floor area* plus *floor* area that is below *grade* and excludes *floor* area devoted to parking.
- 8 *Condominium conversion.* Existing parking located in required front yards shall not be counted toward meeting the required minimum number of parking spaces.

(b) *Eligibility For Shared Parking.* Up to 25 percent of the parking spaces required by this section may be unassigned and eligible for *shared parking* in accordance with Section 142.0545 except that at least one space shall be assigned to each dwelling unit. Within the beach impact area of the Parking Impact Overlay Zone, off- *premises* parking shall not be permitted for residential uses.

(c) *Common Area Parking Requirement.* The common area parking requirement applies to multiple dwelling unit developments that are located in planned urbanizing communities, as identified in the Progress Guide and General Plan, and that are processed in conjunction with a Planned Development Permit. The following standards will be applied by the decision maker when common area parking is required.

(1) The number of common area parking spaces that may be required is 20 percent of the total *off-street parking spaces* required. This requirement may, however, be increased or decreased based on consideration by the decision maker of the following:

(A) For large developments, generally in excess of 200 dwelling units, the number of common area parking may be decreased to no less than 15 percent of the total *off-street parking spaces* required.

(B) In areas where there are few or no on-street parking spaces, where on-street parking spaces are generally occupied, or where on-street parking spaces are not conveniently located, the number of required common area parking spaces may be increased.

(C) For any project with characteristics, surroundings, or expected residents that are likely to have an effect on the demand for common area parking, the number of common area parking spaces may be increased or decreased accordingly.

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- (2) All common area parking that is provided off-street must be clearly identified and reserved for visitors.
- (3) All common area parking that is provided off-street is eligible for shared parking in accordance with Section 142.0545.
- (4) The common area parking requirement may be met on-street by parking spaces that meet the following criteria:
  - (A) The parking spaces completely abut the subject property's street frontage.
  - (B) The parking spaces are within a local street that is improved to City standards to accommodate on-street parking.
  - (C) If the parking spaces are existing, most of them are not usually occupied.
  - (D) On-street parking spaces shall be counted according to on-street parking demarcation or parking meters or, if none exists, as one space per 20 feet of full-height curb.
- (d) Minimum Required Parking Without a 20-foot Driveway. Any *multiple dwelling unit* with a garage that does not provide a driveway that is at least 20 feet long, measured from the back of the sidewalk to that portion of the driveway most distant from the sidewalk, as illustrated in Diagram 142-05A (Section 142.0520), shall provide one additional parking space. This additional parking space may be on-street, abutting the subject property.  
*(Added 12-9-1997 by O-18451 N.S.; amended 10-18-1999 by O-18691 N.S.; effective 1-1-2000.)(Amended 7-5-2006 by O-19506 N.S.; effective 8-4-2006.)*

**§142.0530 Nonresidential Uses — Parking Ratios**

- (a) Retail Sales, Commercial Services, and Mixed-Use Development. Table 142-05D establishes the ratio of required parking spaces to building *floor* area in the commercial zones, industrial zones, and planned districts shown, for retail sales uses and for those commercial service uses that are not covered by Table 142-05E or 142-05F. Table 142-05D also establishes the required parking ratios for mixed-use developments in a single *structure* that include an allowed use from at least two of the following use categories: (1) retail sales, (2) commercial services, and (3) offices.

**Table 142-05D  
Parking Ratios for Retail Sales, Commercial Services, and Mixed-Use Development**

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Zone	Parking Spaces Required per 1,000 Square Feet of Floor Area Unless Otherwise Noted (Floor Area Includes Gross Floor Area plus below Grade Floor Area and Excludes Floor Area Devoted to Parking)			
	Required Automobile Parking Spaces			Required Bicycle Parking Spaces <sup>(2)</sup>
	Minimum Required Outside a Transit Area	Minimum Required Within a Transit Area <sup>(1)</sup>	Maximum Permitted	Minimum Required
<b>Commercial Zones</b>				
CC-1-1 CC-2-1 CC-4-1 CC-5-1	2.5	2.1	6.5	0.1
CC-1-2 CC-2-2 CC-4-2 CC-5-2	2.5	2.1	6.5	0.1
CC-1-3 CC-2-3 CC-4-3 CC-5-3	5.0 <sup>(3)</sup>	4.3	6.5	0.1
CC-3-4 CC-4-4 CC-5-4	2.5	2.1	6.5	0.1
CC-3-5	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	5.5	0.1
CC-3-5/Beach impact area <sup>(5)</sup>	2.5	2.1	6.5	
CC-4-5	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	5.5	
CC-5-5	1.25	1.25	5.5	0.1
CN-1-1	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	5.5	0.1
CN-1-2	5.0	4.3	6.5	0.1
CN-1-3	2.5	2.1	6.5	0.1
CR-1-1 CR-2-1	5.0 <sup>(3)</sup>	4.3	6.5	0.1
CO-1-1 CO-1-2	5.0	4.3	6.5	0.1
CV-1-1	5.0	4.3	6.5	0.1
CV-1-2	2.5	2.1	6.5	0.1
<b>Industrial Zones</b>				
IH-1-1 IH-2-1	5.0	4.3	6.5	0.1
IL-1-1 IL-2-1 IL-3-1	5.0	4.3	6.5	0.1
IP-1-1 IP-2-1	5.0	4.3	6.5	0.1
IS-1-1	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	5.5	0.1
<b>Planned Districts</b>				
Barrio Logan: Subdistrict B			5.5	0.1

	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>		
Barrio Logan: Except Subdistrict B	2.5	2.1	6.5	0.1
Carmel Valley	5.0	4.3	6.5	0.1

Cass Street	2.0	2.0	6.5	0.1
Central Urbanized	2.5	2.1	6.5	0.1
Golden Hill	1.25	1.25	5.5	0.1
La Jolla	1.7	1.7	5.5	0.1
La Jolla Shores	1.0	1.0 <sup>(5)</sup>	5.5	0.1
Mid-City: CN-3 and CV-3	1.25	1.25	5.5	0.1
Mid-City: Except CN-3, CV-3	2.5	2.1	6.5	0.1
Mount Hope	3.3	2.8	6.5	0.1
Mission Valley: CV	2.5	2.1	6.5	0.1
Mission Valley: Except CV	5.0	4.3	6.5	0.1
Otay Mesa	5.0	4.3	6.5	0.1
Old Town	4.0	3.4	6.5	0.1
Southeast San Diego	2.5	2.1	6.5	0.1
San Ysidro	2.5	2.1	6.5	0.1
West Lewis Street	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	5.5	0.1

Footnotes For Table 142-05D

- 1 *Transit Area.* The *transit area* minimum parking ratios apply in the *Transit Area Overlay Zone* (Chapter 13, Article 2, Division 10) and in the *Urban Village Overlay Zone* (Chapter 13, Article 2, Division 11).
- 2 *Bicycle Parking.* See Section 142.0530(e).
- 3 *Uses Located above Ground Floor.* The minimum parking ratio for retail sales and commercial services uses above the ground *floor* is 4.0 spaces per 1,000 square feet of *gross floor* area.
- 4 *Beach impact area.* For area of applicability, see Chapter 13, Article 2, Division 8 (*Parking Impact Overlay Zone*).
- 5 *Alley Access.* For properties with *alley* access, one parking space per 10 linear feet of *alley* frontage may be provided instead of the parking ratio shown in Table 142-05D. Within the beach impact area of the *Parking Impact Overlay Zone*, application of this policy shall not result in a reduction of required on-site parking.

(b) **Eating and Drinking Establishments.** Table 142-05E establishes the required ratio of parking spaces to building *floor* area in the commercial zones, industrial zones, and planned districts shown, for eating and drinking establishments that are the primary use on a *premises*.

Table 142-05E

Parking Ratios for Eating and Drinking Establishments

Zone	Parking Spaces Required per 1,000 Square Feet of Eating and Drinking Establishment <sup>(3)</sup> Floor Area Unless Otherwise Noted (Floor Area Includes Gross Floor Area plus below Grade Floor Area and Excludes Floor Area Devoted to Parking)			
	Required Automobile Parking Spaces			Required Bicycle Parking Spaces <sup>(2)</sup>
	Minimum Required Outside a Transit Area	Minimum Required Within a Transit Area <sup>(1)</sup>	Maximum Permitted	Minimum Required
<b>Commercial Zones</b>				
CC-1-1 CC-2-1 CC-4-1 CC-5-1	2.5	2.1	25.0	0.1
CC-1-2 CC-2-2 CC-4-2	2.5	2.1	25.0	0.1
CC-5-2	2.5	2.1	25.0	0.1
CC-4-2/Coastal Overlay Zone <sup>(4)</sup>	5.0	4.3	25.0	0.1
CC-1-3 CC-2-3 CC-4-3 CC-5-3	15.0	12.8	25.0	0.1
CC-3-4 CC-4-4	2.5	2.1	25.0	0.1
CC-4-4/Coastal Overlay Zone <sup>(4)</sup>	5.0	4.3	25.0	0.1
CC-5-4	2.5	2.1	25.0	0.1
CC-3-5	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	20.0	0.1
CC-3-5/Coastal Overlay Zone <sup>(4)</sup>	5.0	4.3	25.0	0.1
CC-4-5	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	20.0	
CC-5-5	1.25	1.25	20.0	0.1
CN-1-1	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	20.0	0.1
CN-1-2	15.0	12.8	25.0	0.1
CN-1-3	2.5	2.1	25.0	0.1

CR-1-1 CR-2-1	15.0	12.8	25.0	0.1
CO-1-1 CO-1-2	15.0	12.8	25.0	0.1
CV-1-1	15.0	2.1	25.0	0.1
CV-1-2	5.0	4.3	25.0	0.1
<b>Industrial Zones</b>				
IH-1-1 IH-2-1	15.0	12.8	25.0	0.1
IL-1-1 IL-2-1 IL-3-1	15.0	12.8	25.0	0.1
IP-1-1 IP-2-1	15.0	12.8	25.0	0.1
IS-1-1	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	20.0	0.1
<b>Planned Districts</b>				
Barrio Logan: Subdistrict B	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	20.0	0.1
Barrio Logan: Except Subdistrict B	2.5	2.1	20.0	0.1
Carmel Valley	15.0	12.8	25.0	0.1
Cass Street	5.0	4.3	25.0	0.1
Central Urbanized	2.5	2.1	6.5	0.1
Golden Hill	1.25	1.25	20.0	0.1
La Jolla	5.0	4.3	20.0	0.1
La Jolla Shores	1.0	1.0 <sup>(5)</sup>	20.0	0.1
Mid-City: CN-3 and CV-3	1.25	1.25	20.0	0.1
Mid-City: Except CN-3, CV-3	2.5	2.1	25.0	0.1
Mount Hope	3.3	2.8	25.0	0.1
Mission Valley: CV	5.0	4.3	25.0	0.1
Mission Valley: Except CV	15.0	12.8	25.0	0.1
Otay Mesa	15.0	12.8	25.0	0.1
Old Town	4.0	3.4	25.0	0.1
Southeast San Diego	5.0	4.3	25.0	0.1
San Ysidro	5.0	4.3	25.0	0.1
West Lewis Street	1.0 <sup>(5)</sup>	1.0 <sup>(5)</sup>	20.0	0.1



Footnotes For Table 142-05E

- 1 *Transit Area.* The *transit area* minimum parking ratios apply in the *Transit Area Overlay Zone* (Chapter 13, Article 2, Division 10) and in the *Urban Village Overlay Zone* (Chapter 13, Article 2, Division 11).
- 2 *Bicycle Parking.* See Section 142.0530(e)
- 3 *Eating and Drinking Establishments.* The minimum parking ratios apply to eating and drinking establishments that do not have a common parking area with any other uses. There is no minimum parking requirement or maximum permitted parking for outdoor dining. Within the *Coastal Overlay Zone*, outdoor dining areas such as decks, patios, terraces, etc., are considered part of the establishment’s *gross floor area* and included in calculating parking requirements.
- 4 *Coastal Overlay Zone.* For area of applicability, see Chapter 13, Article 2, Division 4.
- 5 *Alley Access.* For properties with *alley* access, one parking space per 10 linear feet of *alley* frontage may be provided instead of the parking ratio shown in Table 142-05E. Within the beach impact area of the *Parking Impact Overlay Zone*, application of this policy shall not result in a reduction of required on-site parking.

- (c) **Nonresidential Uses.** Table 142-05F establishes the required ratio of parking spaces to building *floor* area for the nonresidential uses shown that are not covered by the parking requirements in Section 142.0530(a) and (b).

**Table 142-05F  
Parking Ratios for Specified Non-Residential Uses**

Use	Parking Spaces Required per 1,000 Square Feet of Floor Area Unless Otherwise Noted (Floor Area Includes Gross Floor Area plus below Grade Floor Area, and Excludes Floor Area Devoted to Parking)				
	Required Automobile Parking Spaces				Required Bicycle Parking Spaces <sup>(3)</sup>
	Minimum Required Outside a Transit Area	Minimum Required Within a Transit Area <sup>(1)</sup>	Maximum Permitted	Carpool Minimum <sup>(2)</sup>	Minimum
<b>Institutional</b>					
<b>Separately regulated uses</b>					
Botanical Gardens and Arboretums	3.3	2.8	N/A	N/A	2% of Auto Minimum
Churches and places of religious assembly	1 per 3 seats; or 1 per 60 inches of pew space; or 30 per 1,000 square feet assembly area if seating is not fixed	85% of Minimum	N/A	N/A	2% of Auto Minimum
Educational facilities:					
Kindergarten through grade 9	2.0 per classroom if no assembly area or 30 per 1,000 square feet assembly area	85% of Minimum	N/A	N/A	2% of Auto Minimum
Grade 10 through grade 12	1 per 5 students at maximum occupancy	85% of Minimum	N/A	N/A	2% of Auto Minimum

Vocational/trade schools	1 per student at maximum occupancy	85% of Minimum	N/A	N/A	2% of Auto Minimum
Exhibit Halls & Convention Facilities	1 per 3 seats; 30.0 if no fixed seats	85% of Minimum	N/A	N/A	2% of Auto Minimum
Hospitals	2 per bed	85% of Minimum	N/A	N/A	2% of Auto Minimum
Intermediate care facilities and nursing facilities	1 per 3 beds	85% of Minimum	N/A	N/A	2% of Auto Minimum
Interpretive Centers	3.3	2.8	N/A	N/A	2% of Auto Minimum
Museums	3.3	2.8	N/A	N/A	2% of Auto Minimum
Radio & Television Broadcasting	3.3	2.9	5.0	0.3	0.03 + .03 bike lockers with shower
<b>Retail Sales: See Table 142-05D</b>					
<b>Commercial Services</b>					
<b>Eating &amp; Drinking Establishments</b>	See Table 142-05E				
<b>Public assembly &amp; entertainment</b>					
Theaters	1-3 screens: 1 per 3 seats  4+ screens: 1 per 3.3 seats  Per assembly area if not fixed seats: 50.0	85% of Minimum	N/A	N/A	2% of Auto Minimum
Health clubs	5.0  Clubs with Courts: 1 additional space per the maximum number of authorized players (Amateur Athletic Union) per court	85% of Minimum	N/A	N/A	2% of Auto Minimum
Swimming pools	Commercial: 1 per 100 sq. ft. of pool surface area  Community: 1 per 175 sq. ft. of pool surface area	85% of Minimum	N/A	N/A	2% of Auto Minimum
All other public assembly and entertainment	1 per 3 seats; 30.0 if no fixed seats	85% of Minimum	N/A	N/A	2% of Auto Minimum
<b>Visitor accommodations</b>	1 per <i>guest room</i>  Conference Area: 10.0	1 per <i>guest room</i>  Conference Area: 10.0	N/A	N/A	2% of Auto Minimum
<b>Separately Regulated Uses</b>					
<i>Child Care Facilities</i>	1 per staff	85% of Minimum	N/A	N/A	N/A
Funeral parlors & Mortuaries	1 per 3 seats; 30.0 for assembly area if no fixed seats	85% of minimum	N/A	N/A	2% of Auto Minimum
Outpatient Medical Clinic	4.0	3.5	6.0	0.4	0.03 + .03 bike lockers with shower
Private clubs, lodges, fraternal organizations (except fraternities and sororities)	1 per <i>guest room</i> , or 2.5, whichever is greater <sup>(7)</sup>	85% of Minimum	N/A	N/A	2% of Auto Minimum
Single room occupancy	1 per room	0.5 per room	N/A	N/A	0.2 per room

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hotels	<i>Very low income</i> <sup>(5)</sup> : 0.5 per room	<i>Very low income</i> <sup>(5)</sup> : 0.25 per room			
Veterinary clinics & hospitals	2.5	2.1	N/A	N/A	N/A
<b>Offices</b> <sup>(6)</sup>					
<b>Business &amp; professional/ Government/ Regional &amp; corporate headquarters</b> (except in IS Zone)	3.3	2.9	5.0	0.3	0.03 +.03 bike lockers with shower
<b>Medical, dental, &amp; health practitioners</b> (except in IS Zone)	4.0	3.5	6.0	0.4	0.03 + .03 bike lockers with shower
<b>All office uses</b> in the IS Zone	1.0 <sup>(4)</sup>	1.0 <sup>(4)</sup>	5.0	N/A	0.1
<b>Vehicle &amp; Vehicular Equipment Sales &amp; Service</b>					
<b>Automobile service stations</b>	2 per Station; with Maintenance Facility, 3 per Station Plus 1 per Service Bay  Retail Sales: 3.0	85% of Minimum	N/A	N/A	N/A
<b>Vehicle repair &amp; maintenance</b>	5.0	4.3	N/A	N/A	N/A
<b>Vehicle sales &amp; rentals</b>	1 per each 10 display cars	85% of Minimum	N/A	N/A	N/A
<b>Wholesale, Distribution, and Storage</b> <sup>(6)</sup>					
<b>All wholesale, distribution and storage uses</b>	1.0 <sup>(4)</sup>	1.0 <sup>(4)</sup>	4.0	N/A	0.1
<b>Self Storage Facilities</b>	1.0 space/10,000 sq ft plus 3.3 space per 1,000 square foot of accessory office space	N/A	N/A	N/A	N/A
<b>Industrial</b>					
<b>Heavy Manufacturing</b> (except in IS Zone)	1.5	1.5	4.0	0.2	0.03 + .03 bike lockers with shower
<b>Light manufacturing</b> (except in IS Zone)	2.5	2.1	4.0	0.3	0.03 + .03 bike lockers with shower
<b>Research &amp; development</b> (except in IS Zone)	2.5	2.1	4.0	0.3	0.03 + .03 bike lockers with shower
<b>All industrial uses</b> in the IS Zone	1.0 <sup>(4)</sup>	1.0 <sup>(4)</sup>	4.0	N/A	0.1

Footnotes For Table 142-05F

- 1 *Transit Area.* The *transit area* minimum parking ratios apply in the *Transit Area Overlay Zone* (Chapter 13, Article 2, Division 10) and in the *Urban Village Overlay Zone* (Chapter 13, Article 2, Division 11).
- 2 *Carpool.* See Section 142.0530(d).
- 3 *Bicycle Parking.* See Section 142.0530(e).
- 4 *Alley Access.* For properties with *alley* access, one parking space per 10 linear feet of *alley* frontage may be provided instead of the parking ratio shown in Table 142-05F. Within the beach impact area

of the Parking Impact Overlay Zone, application of this policy shall not result in a reduction of required on-site parking.

- <sup>5</sup> *Very Low Income.* The *very low income* parking ratio applies to dwelling units limited to occupancy by *very low income* households that are covered by an agreement with the San Diego Housing Commission pursuant to Chapter 14, Article 3, Division 7 (Affordable Housing Density Bonus Regulations).
- <sup>6</sup> Accessory Retail Sales, Commercial Services, and Office Uses. On-site accessory retail sales, commercial services, and office uses that are not open to the public are subject to the same parking ratio as the primary use.
- <sup>7</sup> In the beach impact area, one parking space per guest room or 5.0, whichever is greater.

(d) Carpool Spaces

- (1) Required carpool parking spaces are to be provided within the overall minimum parking requirement, not in addition to it.
- (2) Carpool spaces shall be clearly labeled and conveniently located close to employee entrances.
- (3) If there is a charge for parking, carpool spaces shall be offered at a discount for vehicles containing two or more persons.

(e) Bicycle Facilities

- (1) The minimum number of required bicycle parking spaces is two.
- (2) The maximum number of required bicycle parking spaces is 25. These spaces can be accommodated with racks for 25 bicycle spaces or racks for 12 spaces and 12 bicycle lockers if lockers are also required.
- (3) The following uses are exempt from the bicycle facilities requirement:
  - Cemeteries, mausoleums, crematories;
  - Maintenance and repair uses;
  - Boarding kennels;
  - Pet grooming services;
  - Veterinary clinics and hospitals;
  - Vehicle and vehicular equipment sales and service uses; and
  - Mining and extractive industries

- (f) Unspecified Uses. For uses not addressed by Tables 142-05D, 142-05E, and 142-05F the required *off-street parking spaces* are the same as that required for similar uses. The City Manager shall determine if uses are similar.

- (g) Motorcycle Parking. Motorcycle parking shall be provided at a ratio of 2 percent of the minimum number of automobile parking spaces required or two spaces, whichever is greater.

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- (h) Rideshare Information. All nonresidential uses with more than 100,000 square feet of *gross floor* area are required to provide a kiosk or bulletin board that displays information on transit use, carpooling, and other forms of ridesharing.

*(Amended 6-12-2001 by O-18948 N.S.; effective 12-12-2001.)*

*(Amended 3-1-2006 by O-19647 N.S.; effective 8-10-2006.)*

**§142.0535 Off-Premises Parking Regulations in Urbanized Communities**

Required *off-street parking spaces* for uses in urbanized communities as identified in the Progress Guide and General Plan may be located off-premises, subject to the following regulations.

- (a) Residential Uses. Some portion of the off-premises parking shall be within a 25-foot horizontal distance of the *premises* on which the use requiring *off-street parking spaces* is located, and in the Coastal Overlay Zone, the site of the off-premises parking shall be identified with appropriate signs. Within the beach impact area of the Parking Impact Overlay Zone, off-premises parking shall not be permitted for residential uses.
- (b) Nonresidential Uses. Some portion of the off-premises parking shall be within a non-residential zone and within a 600-foot horizontal distance of the *premises* on which the use requiring *off-street parking spaces* is located, and in the Coastal Overlay Zone, the site of the off-premises parking and the site of the use shall be identified with appropriate signs within the Coastal Overlay Zone.
- (c) Control of Parking Spaces. The off-premises parking, which shall be identified with appropriate directional signs for *development* in the Coastal Overlay Zone, shall be owned or controlled by the owner of the use requiring the *off-street parking spaces*. When off-premises parking is to be provided, the owner or lessee of record of the *premises* shall furnish evidence that is satisfactory to the City Manager that they own or have a sufficient interest in the property to provide the minimum *off-street parking spaces* required by the Land Development Code. Whether *off-street parking spaces* are to be provided on property that is owned by the *applicant* or another owner, the *applicant* shall provide to the County Recorder for recordation, covenants that have been executed by the owners of the property on which the off-premises parking is proposed and the owners of the use requiring the off-street parking spaces. The covenant shall be for the benefit of the City, in a form approved by the City Attorney, to the effect that the owners will continue to maintain the parking spaces as long as the use it serves exists. The covenant shall also recite that the title to, and right to, use the *lots* upon which the parking is to be provided will be subservient to the title to the *premises* where the *primary use* it serves is situated and shall warrant that the *lots* are not and will not be made subject to any other covenant or contract for use without prior written consent of the City. If the owners of the use should thereafter provide parking equal in area within the same distance and under the same conditions as the ownership upon another *lot* than the *premises* made subservient in a previous covenant, the City will, upon written application accompanied by the filing of a similar covenant, release the original subservient *premises* from the previous covenant. The owners shall furnish at their own expense title reports

or other evidence the City may require to insure compliance with the provisions of this section.

- (d) **Off-Premises *Parking Spaces* Not to be Reduced.** Off-premises *parking spaces* provided in accordance with this section shall be maintained as long as the *structure* or use for which they are provided exists unless an equivalent substitute number of spaces is provided and thereafter maintained in compliance with the Land Development Code. In no event shall parking facilities that are provided or maintained for a *structure* or use be considered as providing any of the required space for any other *structure* or use.

*(Added 12-9-1997 by O-18451 N.S.; amended 10-18-1999 by O-18691 N.S.; effective 1-1-2000.)*

**§142.0540 Exceptions to Parking Regulations for Nonresidential Uses**

- (a) **Commercial Uses on Small Lots.** Outside the beach impact area of the Parking Impact Overlay Zone, for *lots* that are 7,000 square feet or less, that existed before January 1, 2000, including abutting *lots* under common ownership, the parking requirements set forth in Table 142-05G may be applied to all commercial uses at the option of the *applicant* as an alternative to the requirements set forth in Section 142.0530. The type of access listed in Table 142-05G determines the minimum number of required *off-street parking spaces*.

**Table 142-05G  
Alternative Parking Requirement for  
Commercial Uses on Small Lots**

<b>Type of Access</b>	<b>Minimum Number of Parking Spaces</b>
With <i>Alley Access</i> <sup>(1)</sup>	1 space per 10 feet of <i>alley</i> frontage, minus one space
Without <i>Alley Access</i>	none required

Footnote to Table 142-05G

<sup>1</sup> The City Engineer will determine whether a *lot* has adequate *alley* access according to accepted engineering practices.

- (b) **Exceeding Maximum Permitted Parking.** Development proposals may exceed the maximum permitted automobile parking requirement shown in Tables 142-05D, 142-05E, and 142-05F with the approval of a Neighborhood Development Permit, subject to the following:

- (1) The *applicant* must show that the proposed parking spaces are required to meet anticipated parking demand, will not encourage

additional automobile trips, and will not result in adverse site design impacts; and

- (2) The number of automobile parking spaces provided shall not be greater than 125 percent of the maximum that would otherwise be permitted.
- (c) Varying From Minimum Parking Requirements. Development proposals may, at the applicant's option, vary from the minimum parking requirements of this division with the approval of a Transportation Demand Management (TDM) Plan and Site Development Permit decided in accordance with Process Three, subject to the following requirements.
  - (1) The TDM Plan shall be designed to reduce peak period automobile use with such techniques as carpooling, vanpooling, transit, bicycling, walking, telecommuting, compressed work weeks, or flextime.
  - (2) To compensate for a reduction in parking, the TDM Plan shall specify only those measures that would not otherwise be required by this division.
  - (3) In no case shall the number of automobile parking spaces provided be less than 85 percent of the minimum that would otherwise be required.
  - (4) The *applicant* shall show that the TDM Plan adequately mitigates the proposed reductions in automobile parking.
  - (5) The owner shall set aside land for a parking facility or allow for future construction or expansion of a structured parking facility that is sufficient to provide additional parking spaces equal in number to the number reduced.
  - (6) In the event of noncompliance with the TDM Plan, the City Manager shall require the owner to construct additional parking spaces equal in number to the spaces originally reduced.

*(Added 12-9-1997 by O-18451 N.S.; amended 10-18-1999 by O-18691 N.S.; effective 1-1-2000.)*



**§142.0545 Shared Parking Requirements**

- (a) Approval Criteria. In all zones except single unit residential zones, *shared parking* may be approved through a Building Permit subject to the following requirements.
  - (1) *Shared parking* requests shall be for two or more different land uses located adjacent or near to one another, subject to the standards in this section.
  - (2) All *shared parking* facilities shall be located within a 600-foot horizontal distance of the uses served.
  - (3) Parties involved in the shared use of a parking facility shall provide an agreement for the shared use in a form that is acceptable to the City Attorney.
  - (4) *Shared parking* facilities shall provide *signs* on the *premises* indicating the availability of the facility for patrons of the participating uses.
  - (5) Modifications to the *structure* in which the uses are located or changes in tenant occupancy require review by the City Manager for compliance with this section.
- (b) Shared Parking Formula. *Shared parking* is based upon the variations in the number of parking spaces needed (parking demand) over the course of the day for each of the proposed uses. The hour in which the highest number of parking spaces is needed (peak parking demand) for the proposed *development*, based upon the standards in this section, determines the minimum number of required *off-street parking spaces* for the proposed *development*.

(1) The *shared parking* formula is as follows:

- A, B, C = proposed uses to share parking spaces
- PA = parking demand in the peak hour for Use A
- PB = parking demand in the peak hour for Use B
- PC = parking demand in the peak hour for Use C
- HA% = the percentage of peak parking demand for Use A in Hour H
- HB% = the percentage of peak parking demand for Use B in Hour H
- HC% = the percentage of peak parking demand for Use C in Hour H
- P(A, B, C) = peak parking demand for Uses A, B and C combined

Formula:

$$P(A, B, C) = (PA \times HA\%) + (PB \times HB\%) + (PC \times HC\%), \text{ where } H = \text{that hour of the day (H) that maximizes}$$

P(A, B, C)

- (2) Table 142-05G contains the peak parking demand for selected uses, expressed as a ratio of parking spaces to *floor* area.
- (3) Table 142-05I contains the percentage of peak parking demand that selected uses generate for each hour of the day (hourly accumulation curve), in some cases separated into weekdays and Saturdays. The period during which a use is expected to generate its peak parking demand is indicated as 100 percent, and the period during which no parking demand is expected is indicated with “-”.
- (4) The parking demand that a use generates in a particular hour of the day is determined by multiplying the peak parking demand for the use by the percentage of peak parking demand the use generates in that hour.

- (5) The parking demand of the proposed *development* in a particular hour of the day is determined by adding together the parking demand for each use in that hour.
  - (6) The minimum number of required *off-street parking spaces* for the proposed *development* is the highest hourly parking demand.
  - (7) Uses for which standards are not provided in Tables 142-05H and 142-05I may nevertheless provide *shared parking* with the approval of a Neighborhood Development Permit, provided that the *applicant* shows evidence that the standards used for the proposed *development* result in an accurate representation of the peak parking demand.
- (c) Single Use Parking Ratios. *Shared parking* is subject to the parking ratios in Table 142-05H.

**Table 142-05H  
Parking Ratios for Shared Parking**

Use	Peak Parking Demand (Ratio of spaces per 1,000 square feet of floor area unless otherwise noted. Floor area includes gross floor area plus below grade <i>floor</i> area and excludes floor area devoted to parking)	Transit Area <sup>(1)</sup>
<b>Office</b> (except medical office)		
Weekday	3.3	2.8
Saturday	0.5	0.5
<b>Medical office</b>		
Weekday	4.0	3.4
Saturday	0.5	0.5
<b>Retail sales</b>	5.0	4.3
<b>Eating &amp; drinking establishment</b>	15.0	12.8
<b>Cinema</b> 1-3 screens 4 or more screens	1 space per 3 seats 1 space per 3.3 seats	.85 spaces per 3 seats .85 spaces per 3.3 seats

<b>Visitor accommodations through Multiple Dwelling Units</b>	1 space per <i>guest room</i>	1 space per <i>guest room</i>
<b>Conference room</b>	10.0	10.0
<b>Multiple dwelling units</b>	(see Section 142.0525)	

Footnote for Table 142-05H

<sup>1</sup> *Transit Area.* The *transit area* peak parking demand applies in the *Transit Area* Overlay Zone (see Chapter 13, Article 2, Division 10).

- (d) Hourly Accumulation Rates. Table 142-05I contains, for each hour of the day shown in the left column, the percentage of peak demand for each of the uses, separated in some cases into weekdays and Saturdays.

**Table 142-05I  
Representative Hourly Accumulation by Percentage of Peak Hour**

Hour of Day	Office (Except Medical Office)		Medical Office		Retail Sales		Eating & Drinking establishment.		Cinema	
	Weekday	Saturday	Weekday	Saturday	Weekday	Saturday	Weekday	Saturday	Weekday	Saturday
6 a.m.	5%	-	5%	-	-	-	15%	20%	-	-
7 a.m.	15	30%	20	20%	10%	5%	55%	35%	-	-
8 a.m.	55	50	65	40	30	30	80	55	-	-
9 a.m.	90	80	90	80	50	50	65	70	-	-
10 a.m.	100	90	100	95	70	75	25	30	5%	-
11 a.m.	100	100	100	100	80	90	65	40	5	-
Noon	90	100	80	100	100	95	100	60	30	30%
1 p.m.	85	85	65	95	95	100	80	65	70	70
2 p.m.	90	75	80	85	85	100	55	60	70	70
3 p.m.	90	70	80	95	80	90	35	60	70	70
4 p.m.	85	65	80	50	75	85	30	50	70	70
5 p.m.	55	40	50	45	80	75	45	65	70	70
6 p.m.	25	35	15	45	80	65	65	85	80	80

7 p.m.	15	25	10	40	75	60	55	100	100	90
8 p.m.	5	20	5	5	60	55	55	100	100	100
9 p.m.	5	-	5	-	45	45	45	85	100	100
10 p.m.	5	-	5	-	30	35	35	75	100	100
11 p.m.	-	-	-	-	15	15	15	30	80	80
Midnight	-	-	-	-	-	-	5	25	70	70

Hour of Day	Visitor Accommodations					
	<i>Guest Room</i>		Eating & Drinking Establishment		Conference Room	Exhibit Hall and Convention Facility
	Weekday	Saturday	Weekday	Saturday	Daily	Daily
6 a.m.	100%	90%	15%	20%	-	-
7 a.m.	95	80	55	35	--	-
8 a.m.	85	75	80	55	50%	50%
9 a.m.	85	70	65	70	100	100
10 a.m.	80	60	25	30	100	100
11 a.m.	75	55	65	40	100	100
Noon	70	50	100	60	100	100
1 p.m.	70	50	80	65	100	100
2 p.m.	70	50	55	60	100	100
3 p.m.	60	50	40	60	100	100
4 p.m.	65	50	30	50	100	100
5 p.m.	60	60	45	65	100	100
6 p.m.	65	65	65	85	100	100
7 p.m.	75	70	55	100	100	100
8 p.m.	85	70	55	100	100	100
9 p.m.	90	75	45	85	100	100
10p.m.	90	85	35	75	50	50
11p.m.	100	95	15	30	-	-

Midnight	100	100	10	25	-	-
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Hour of Day	Residential	
	Weekday	Saturday
6 a.m.	100%	100%
7 a.m.	80	100
8 a.m.	60	95
9 a.m.	50	85
10 a.m.	40	80
11 a.m.	40	75
Noon	40	70
1 p.m.	35	65
2 p.m.	40	65
3 p.m.	45	65
4 p.m.	45	65
5 p.m.	50	65
6 p.m.	65	70
7 p.m.	70	75
8 p.m.	75	80
9 p.m.	85	80
10 p.m.	90	85
11 p.m.	95	90
Midnight	100	95

*(Added 12-9-1997 by O-18451 N.S.; effective 1-1-2000.)  
(Amended 3-1-2006 by O-19467 N.S.; effective 8-10-2006.)*

**§142.0550 Parking Assessment District Calculation Exception**

- (a) Exemption From Minimum Required Parking Spaces. Property within a parking assessment district formed pursuant to any parking district ordinance adopted by the City Council may reduce the number of parking spaces provided from the minimum automobile space requirements in Tables

142-05C, 142-05D, 142-05E, and 142-05F in accordance with the application of the following formula:

(Assessment against the subject property) / (Total assessment against all property in the parking district) x (parking spaces provided in the district facility) x 1.25 = parking spaces reduced.

The remainder of the *off-street parking spaces* required by Tables 142-05C, 142-05D, 142-05E, and 142-05F shall be provided on the *premises* or as otherwise provided in the applicable zone.

- (b) Property Within More than One Parking Assessment District. Property located in more than one parking assessment district is entitled to the exemption provided in Section 142.0550(a) for each parking assessment district.

*(Added 12-9-1997 by O-18451 N.S.; effective 1-1-2000.)*

**§142.0555 Tandem Parking Regulations**

- (a) Tandem Parking for Residential Uses. Tandem parking regulations for residential uses are described in Chapter 13, Article 2, Division 9 (Residential Tandem Parking Overlay Zone).
- (b) Tandem Parking for Commercial Uses. Tandem parking for commercial uses may be approved through a Neighborhood Development Permit provided the tandem parking is limited to the following purposes:
  - (1) Assigned employee parking spaces;
  - (2) Valet parking associated with restaurant use; and
  - (3) Bed and breakfast establishments.

*(Added 12-9-1997 by O-18451 N.S.; amended 10-18-1999 by O-18691 N.S.; effective 1-1-2000.)*

**§142.0560 Development and Design Regulations for Parking Facilities**

- (a) General Regulations for Parking Areas
  - (1) In computing the required number of *off-street parking spaces* and bicycle spaces, a remaining fraction of one-half or more parking space is deemed a whole parking space; a remaining fraction of less than one-half is disregarded.
  - (2) For mixed uses on the same *premises*, the required parking spaces shall be either of the following:
    - (A) The sum of the requirements for each individual use computed separately; or
    - (B) In compliance with Section 142.0545 *shared parking* requirements.



- (3) Where five or more required spaces are provided on a *premises*, each space shall be clearly delineated with paint or other more durable material contrasting in color with the surface to which it is applied.
  - (4) Parking areas with fewer than 5 spaces are not required to be delineated. The number of spaces in undelineated parking areas shall be determined by dividing the square footage of the usable parking area by 350.
- (b) Minimum Dimensions for *Off-street Parking Spaces*. The minimum dimensions for single and tandem spaces for specific types of parking spaces are shown in Table 142-05J , except as provided in Section 142.0560(e) for certain pre-existing parking facilities. Compact spaces are not permitted.

**Table 142-05J**  
**Minimum Off-Street Parking Space Dimensions**

Type of Space	Required Single Space Dimensions	Required Tandem Space Dimensions
<b>Parking space unobstructed:</b> Retail sales uses and eating and drinking establishments  All other uses	8'-3" wide x 18' long  8' wide x 18' long	All uses 8' wide x 36' long
<b>Parking space abuts a wall, column, or other immovable obstacle</b>	One side abutting obstacle: 9' wide x 18' long  Two sides abutting obstacle: 9½' wide x 18' long	One side abutting obstacle: 9' wide x 36' long  Two sides abutting obstacle: 9½' wide x 36' long
Garage door behind space	Add 1 foot to the required parking space length	Add 1 foot to the required parking space length
Garage door between front and rear space	N/A	Add 5 feet to the required parking space length
<b>Parking Space parallel to aisle (interior space)</b>	8' wide x 21' long	N/A

- (c) Minimum Dimensions for Automobile Parking Aisles. The minimum dimensions for automobile parking aisles at permitted angles for one-way and two-way circulation are shown in Table 142-05K and illustrated in Diagram 142-05B, except as provided in Section 142.0560(e) for certain pre-existing parking facilities.

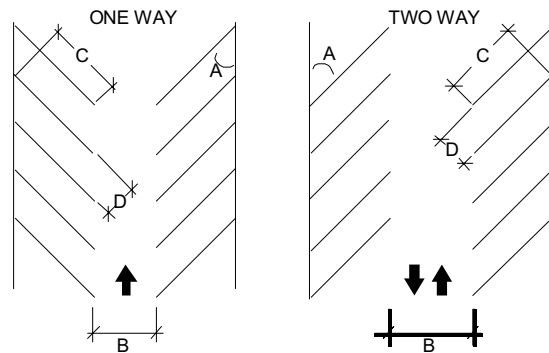
**Table 142-05K  
Aisle Dimensions**

Angle Between Parking Space and Aisle	Minimum Required Aisle Width (feet)	
	One Way	Two Way
90° (perpendicular)	24 <sup>1</sup>	24 <sup>1</sup>
75°	23	24
60°	18	22
45°	12	20
0° (parallel)	12	20

<sup>1</sup>For narrow lots 50 feet or less in width, the minimum drive aisle may be reduced to 22 feet.

**Diagram 142-05B**

**Minimum Dimensions for Automobile Parking Spaces and Aisles**



- A. Angle between parking space and aisle
- B. Aisle width
- C. Space length: 18'
- D. Space width: 8'3" retail sales and restaurant uses.  
(all other uses 8')

- (1) For other angles between 45 and 90 degrees, use the aisle width for the next larger angle in Table 142-05K.

- (2) Parking spaces that are not parallel to the aisle (0 degrees between the parking space and aisle) shall be at an angle of at least 45 degrees to the aisle, except as otherwise approved by the City Manager according to accepted engineering practices.
- (d) Vehicular Circulation within Parking Facilities
  - (1) Each parking space shall open directly on an aisle or driveway of such width and design as to provide safe and efficient means of vehicular access to the parking space. All required parking facilities shall have convenient and direct access to a public *street* or *alley*.
  - (2) Vehicular circulation from one aisle to another on the same *premises* shall not require the use of a public *street*.
  - (3) Aisles that do not provide through circulation shall provide a turnaround area at the end of the aisle that is clearly marked to prohibit parking and that has a minimum area equivalent to a parking space.
- (e) Minimum Parking and Aisle Dimensions for Pre-existing Parking Facilities. Required *off-street parking spaces* approved before January 1, 2000 need not be restriped to comply with this section when enlargement or a change in use is undertaken if the spaces required for both the existing use and enlargement comply with the following standards:
  - (1) The parking stall dimensions for 90-degree parking are 8 feet, 6 inches wide by 20 feet long with 21-foot aisles; or
  - (2) A maximum of 60 percent of the total number of spaces may be 7 feet, 6 inches wide by 15 feet long with 18-foot aisles..
- (f) Minimum Dimensions for Motorcycle Parking Facilities. Motorcycle spaces shall be at least 3 feet wide and 8 feet long.
- (g) Minimum Bicycle Facilities. Bicycle racks, bicycle lockers, and shower facilities required by this division shall comply with the following.
  - (1) A bicycle rack is not required for bicycle parking spaces required for a dwelling unit with a garage that is for use only by the occupants of the dwelling unit.

- (2) Bicycle racks used to provide required bicycle parking spaces shall allow the locking of both wheels and the frame without the use of chains or cables.
  - (3) Bicycle lockers may be substituted by enclosed, locked, limited-access areas with racks.
  - (4) Bicycle racks and lockers shall be located in convenient, visible, well-lit areas that are not accessible by motor vehicles or within a pedestrian path. They may be located within the *public right-of-way* only upon approval of the City Manager, according to accepted engineering practices.
  - (5) Shower facilities shall include lockers and shall be free-of-charge to bicycle commuters.
- (h) Surface Parking Design Regulations
- (1) All parking spaces, parking areas, loading areas, and driveways shall be surfaced with asphaltic concrete at least 2 inches in depth or its equivalent as determined by the City Manager according to accepted engineering practices, except that vehicles and equipment permitted to be stored in accordance with Chapter 14, Article 2, Division 11 (Outdoor Storage, Display, and Activity Regulations), may be placed on a permeable surface. Surfacing shall be placed on a suitable compacted and prepared base.
  - (2) Any artificial lighting of parking areas shall be shaded so as not to fall on adjacent properties.
  - (3) All surface parking facilities (including aisles and driveways) in a *multiple dwelling unit*, commercial, or industrial zone containing five or more parking spaces shall be bounded by walls, *solid fences*, buildings, landscaping, or a combination of these whenever the parking area abuts residentially zoned property along the side or rear *property lines* of the *premises* on which the parking area is located. All walls, buildings, and landscaping shall be in compliance with applicable *setback* and *fence* requirements.
  - (4) On any *premises* containing five or more parking spaces, all parking spaces within 10 feet of the boundaries of abutting properties shall be equipped with curbs or permanently anchored wheel stops not less substantial than a 6-inch by 6-inch timber. Wheel stops shall be at least 6 feet in length and located to provide a vehicle overhang of not more than 3 feet.

- (5) The length of a parking space may overhang a raised curb or wheel stop and a required landscape area by a maximum of 3 feet, provided that the height of the curb does not exceed 6 inches and the area of overhang is not part of a required pedestrian path.
- (6) The maximum gradient in any direction within a surface parking facility shall be 6 percent, except that where unusual or special circumstances warrant, the City Manager may approve steeper gradients according to accepted engineering practices.
- (i) Queue Requirements for Drive-Up Service. Queue space for a minimum of five cars shall be provided for each drive-up service window or position. The queue space for each car shall be 10 feet wide and 20 feet long. Required queue spaces shall not obstruct access to parking aisles or parking spaces.
- (j) Driveway and Access Regulations
  - (1) Driveway width shall be determined based on the size of the lot, type of use proposed, and location inside or outside of the Beach Impact Area of the Parking Impact Overlay Zone. Refer to Tables 142-05L and 142-05M for the applicable minimum and maximum driveway widths.

**Table 142-05L  
Driveway Width (Lots greater than 50 feet in width)**

Use	Minimum Width		Maximum Width (Outside of Beach Impact Area of the Parking Impact Overlay Zone)		Maximum Width Beach Impact Area
	One-Way	Two-Way	One-Way	Two-Way	
<i>Detached Single Dwelling Unit</i>	12 feet		25 feet		12 feet
<i>Dwelling Unit in the RX Zone</i>	12 feet		20 feet		12 feet
<i>Multiple Dwelling Unit</i>	14 feet	20 feet	20 feet	25 feet	20 feet
Nonresidential	14 feet	24 feet	20 feet	30 feet	25 feet

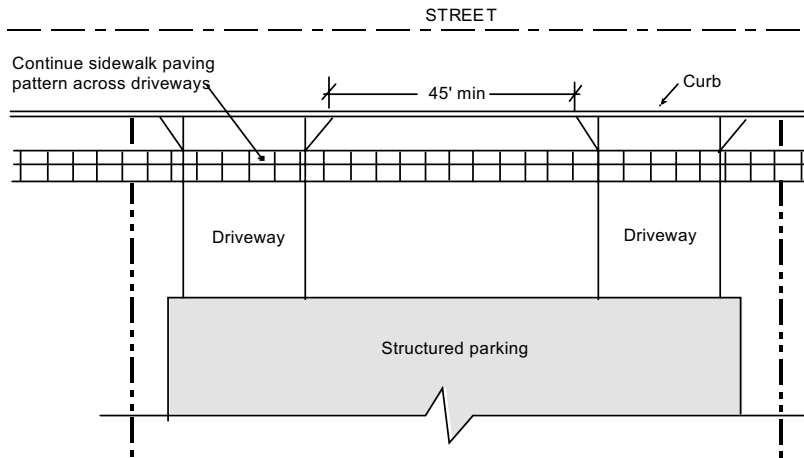
**Table 142-05M**  
**Driveway Width (Lots 50 feet or less in width)**

Use	Minimum Width		Maximum Width (Outside of Beach Impact Area of the Parking Impact Overlay Zone)		Maximum Width Beach Impact Area
	One-Way	Two-Way	One-Way	Two-Way	
Detached <i>Single Dwelling Unit</i>	12 feet		20 feet		12 feet
<i>Dwelling Unit</i> in the RX Zone	12 feet		20 feet		12 feet
<i>Multiple Dwelling Unit</i>					
2 units	12 feet		20 feet		12 feet
3-5 units	14 feet		20 feet		14 feet
6 or more units	14 feet	20 feet	20 feet	25 feet	20 feet

- (2) When redevelopment occurs, after the demolition of the primary *structures* on a *premises*, existing driveways shall be modified to comply with this section.
- (3) Driveway openings shall comply with San Diego Regional Standard Drawing G-16 and either Number G-14A and G-14B, Concrete Driveways, or SDG-114, Concrete Driveway Commercial Alternate, except that driveway openings abutting a through travel lane less than 17 feet wide with an existing or anticipated speed limit of 30 miles per hour or greater shall conform to Drawing Number SDG-114, Concrete Driveway Commercial Alternate.
- (4) Driveways that serve as direct access to *off-street parking spaces* and that traverse a sidewalk or curb shall be at least 20 feet long measured from the back of the sidewalk to that portion of the driveway most distant from the sidewalk, as illustrated in Diagram 142-05A in Section 142.0520.
- (5) Driveway entrances crossing a sidewalk shall maintain the scoring pattern and color used in the adjacent sidewalk areas, as illustrated in Diagram 142-05C. Special paving requires the approval of the City Engineer.
- (6) There shall be at least a 45-foot length of full-height curb between driveways serving the same *premises*, as illustrated in Diagram 142-05C.

Diagram 142-05C

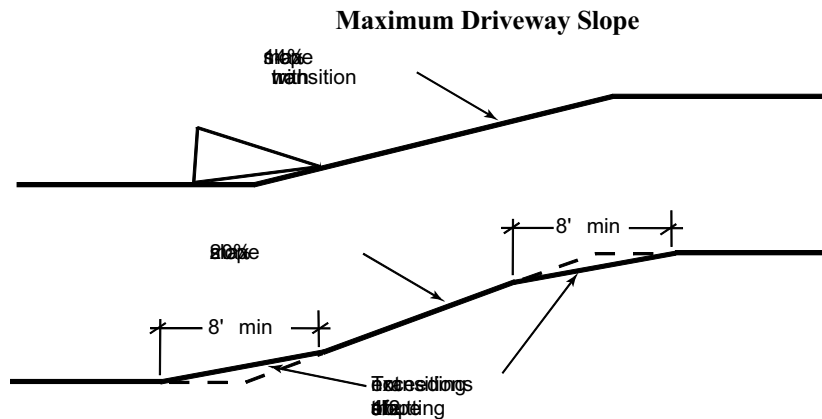
Driveway Spacing and Scoring Pattern



- (7) Within the beach impact area of the Parking Impact Overlay Zone, as shown in Chapter 13, Article 2, Division 8, where any new *development* or any redevelopment will increase the *gross floor area* by 50 percent or more on a *premises* abutting an improved *alley*, the required *off-street parking spaces* shall be accessible from the *alley*.
- (8) Maximum Number of Driveways Permitted on a *Premises*.
  - (A) For properties with no access to an *alley*, there shall be at least one driveway opening permitted per *street frontage* with a maximum of one driveway opening for each 100 feet of *street frontage*.
  - (B) For properties with access to an *alley* and at least 150 feet of total *street frontage*, a maximum of one driveway opening for each 150 feet of frontage is permitted.
  - (C) For properties with access to an *alley* and less than 150 feet of total frontage, a driveway is not permitted, except that in the RM-1-1, RM-1-2, and RM-1-3 zones, one driveway may be permitted if the prohibition of a driveway opening would preclude achieving the maximum *density* permitted by the underlying zone.
- (9) Driveway Gradient Regulations
  - (A) Driveways may be up to 5 percent gradient with no transitions.

- (B) Between the driveway apron and any driveway gradient greater than 5 percent, there shall be a 20-foot-long flat transition not exceeding a 5 percent gradient. A shorter transition may be approved by the City Manager according to accepted engineering practices.
- (C) For driveways with a gradient greater than 14 percent up to the maximum permitted gradient of 20 percent, there shall be transitions for the first and last 8 feet of the ramp. The transitions shall not exceed one-half of the abutting slope of the driveway, as illustrated in Diagram 142-05D.

Diagram 142-05D



- (10) All driveways shall lead to a legal, off-street parking area or loading area on the same premises or to legal parking on neighboring property, if permitted by variance, recorded map easement, or other approved mechanism.
- (k) Parking Structure Design Regulations. Parking structures are subject to the following design regulations:
  - (1) The perimeter of each parking garage floor above street level shall have an opaque screen or other screening mechanism to shield automobiles from public view. The screen shall be at least 3½ feet high measured from the finished floor elevation. An architectural treatment, such as a finished fascia, shall be provided to shield any unfinished structural elements (including electrical elements, exposed metal beams, and fireproofing material) or mechanical appurtenances from a viewing position at grade from the opposite side of the street. Lights visible from the exterior of the structure shall be covered or screened with a diffusing lens and oriented to minimize the visual impact from a viewing position at grade from the opposite side of the street. The top floor of parking structures that are open to the sky are



subject to the vehicular use area requirements of the Landscape Regulations (Chapter 14, Article 2, Division 4).

- (2) The maximum gradient in any direction within a parking *structure* is 6 percent, except that where unusual or special circumstances warrant, the City Manager may approve steeper gradients according to accepted engineering practices.  
*(Amended 1-9-2001 by O-18910 N.S.; effective 8-8-2001.)*  
*(Amended 11-28-2005 by O-19444 N.S.; effective 2-9-2006.)*  
*(Amended 3-1-2006 by O-19467 N.S.; effective 8-10-2006.)*